

## ADDENDUM No. 1

### MEADOWBROOK ESTATES STORMWATER IMPROVEMENTS

Project No. 2016-201-1

COLE COUNTY DEPARTMENT OF PUBLIC WORKS  
February 26, 2018

**Notice is hereby given to bidders** that in reference to the subject project, the following modifications shall be noted. All other aspects of the Contract Documents remain in full force and effect.

#### **BID PROPOSAL FORM CHANGES**

The following changes have been made to the bid proposal form. **A revised bid proposal form is attached.** The Bidder shall use the revised bid proposal form when preparing his bid.

- Added Item 401-99.09 – Bituminous Pavement – Variable Depth, 431 Tons
- Changed Quantity of Item 401-99.07 – Bituminous Pavement – 1.5" Surface to 15,537 SY
- Changed Quantity of Item 413-60.00 – PCC Joint/Crack Seal to 7,202 LF
- Added Add Alternate B with Item 413.60.00 – PCC Joint/Crack Seal, 2,950 LF

#### **JOB SPECIAL PROVISIONS CHANGES:**

The Bidder shall replace the following Job Special Provisions with the ones listed below, changes are denoted with underline.

#### B. WORK ZONE TRAFFIC MANAGEMENT PLAN

**1.0 Description.** Work zone traffic management supplied by the contractor shall be in accordance with applicable portions of Division 100 and Division 600 of the Standard Specifications, and specifically as follows.

#### **2.0 Traffic Management Schedule.**

**2.1** Traffic management schedules shall be submitted to the engineer for review prior to the start of work and prior to any revisions to the traffic management schedule. The traffic management schedule shall include the proposed traffic control measures, hours traffic control will be in place, and work hours.

**2.2** The contractor shall notify the engineer **THREE WEEKS** prior to road closures or shifting traffic onto detours.

**2.3** The engineer shall be notified as soon as practical of any postponement due to weather, material or other circumstances.

**2.4** In order to ensure minimal traffic interference, the contractor shall schedule lane closures for the absolute minimum amount of time required to complete the work. Lanes shall not be

closed until material is available for continuous construction and the contractor is prepared to diligently pursue the work until the closed lane is opened to traffic.

**2.5 Traffic Congestion.** The contractor shall, upon approval of the engineer, take proactive measures to reduce traffic congestion in the work zone.

**2.5.1 Traffic Delay.** The contractor shall be responsible for maintaining the existing local traffic flow through the job site during construction. If disruption of the traffic flow occurs and traffic is backed up in queues of 15 minute delays or longer, then the contractor shall review the construction operations which contributed directly to disruption of the traffic flow and make adjustments to the operations to prevent the queues from occurring again.

**2.5.2 Traffic Safety.**

**2.5.2.1** Where traffic queues routinely extend to within 1000 feet (300 m) of the ROAD WORK AHEAD, or similar, sign on a divided highway or to within 500 feet (150 m) of the ROAD WORK AHEAD, or similar, sign on an undivided highway, the contractor shall contact the County for an evaluation of the traffic control.

**3.0 Work Hour Restrictions:** There are no work hour restrictions for this project. It shall be the responsibility of the engineer to determine if work hours may be modified. Working hours for evenings, weekends and holidays will be determined by the engineer.

**4.0 Detours and Lane Closures.**

**4.1** The contractor shall notify the emergency response agencies prior to any closures. The contractor shall submit a schedule of closure to the engineer **THREE WEEKS** prior to the closure. The engineer must approve the schedule prior to any work beginning.

**4.2** Idlewood shall remain open at all times through the use of flagger control or other methods as approved by the engineer. The local roads may be closed to thru traffic during construction with the approval of the engineer, but local traffic must have access to the area at all times.

**4.3** When closing the road, the contractor shall place Type III Movable Barricades with R11-2 signs and Type C warning lights at the road closure. The contractor shall also place W020-1 and R11-3a signs in advance of any road work or closure. The signs shall be in accordance with MoDOT's Engineering Policy Guide and approved by the Engineer. If the contractor has to do any miscellaneous work before and/or after the road closures, he shall provide temporary traffic control in accordance with the Manual on Uniform Traffic Control Devices. This shall include but not be limited to, signing, flagging, cones, etc.

**4.4** The Contractor shall install detour signing as determined by their phasing plan and approved by the field engineer.

**4.5** Where one lane of the road is closed, the contractor shall utilize a flagger to maintain traffic on the adjacent lane unless otherwise approved by the field engineer.

**5.0 Property Owner Coordination:** Contractor must coordinate with adjacent property owners to schedule construction of the entrance. Contractor shall provide property owners access to their property at all times by use of phased construction, shared entrances, temporary entrances, or other means necessary. Property access closures may be allowed with permission of property owner and approval of the Engineer. The contractor shall be aware that the property owner at 2404 Cheryl Drive will need access maintained at all times in a manner acceptable under Americans with Disabilities Act (ADA) guidelines.

**6.0 Basis of Payment:** Any expenses incurred by the contractor by reason of their compliance with this provision shall be considered as completely covered by project bid items.

#### H. EXCAVATION FOR WATERLINE REPLACEMENT

**1.0 Description.** This work involves excavation for the replacement of the waterlines along Cheryl Drive and Lake Valley Drive owned by Public Water District #2. Work includes excavation and backfill for the relocated line on the south of Cheryl from Idlewood Road to west of the box culvert crossing and on the east of Lake Valley in the vicinity of the box culvert crossing.

**2.0 Construction.** The contractor will be responsible for excavating for the new waterline alignment in both locations, and coordinating the construction and backfill of the completed waterline with the Water District. All materials and labor for the waterline shall be provided by the Water District. The excavation along Cheryl is expected to parallel the storm sewer system to the west of Christine, and shall be behind the south curb line to the east of Christine.

**3.0 Materials.** The Public Water Service District #2 will supply all the materials such as pipes, valves and fittings, concrete for thrust blocks, and aggregate backfill. The water district will also perform the installation and connections with no assistance from the contractor. The contractor shall be responsible for all excavation and backfill required for the waterline, as well as any utility locates or coordination which may be necessary.

**4.0 Basis of Payment.** Any expenses incurred by the contractor by reason of their compliance with this provision shall be considered as completely covered by Item No. 206-99.03, "Waterline Excavation", per lineal foot. No adjustments will be made for rock encountered. The Bidder shall include the additional Job Special Provisions listed below.

#### Q. DRAINAGE

**1.0 Description:** This work consists of providing a storm sewer consisting of pipes, inlets, manholes, and flared end sections.

**2.0 Requirements:** Dimensions and details of the inlets and manholes shall be according to the City of Jefferson Standard Details.

**2.1** The construction and materials for the concrete drop inlets shall conform to Section 731. The inlets shall be cast-in-place to allow for field adjustments as needed. The manholes and junction structures may be precast, but the contractor shall be aware that field adjustments may be necessary.

**3.0 Method of Measurement:** No measurements will be made and contract quantity will be used unless there is a change in construction or an error found in the contract quantity.

**4.0 Basis of Payment:** Excavation and backfill shall be included with the price for the pipes and inlets. There will be no direct payment for excavation and backfill for the drainage structures, unless rock is encountered. If rock is encountered, the contractor's additional expense will be paid by Item No. 206-31.00 "Class 3 Excavation in Rock", per cubic yard. This additional payment shall only include the contractor's cost to chip the rock as typical excavation and backfill procedures will be included in the cost of the drainage structures. Rock shall be considered Class C Excavation material as defined in Section 203.2.

**4.2** All expenses incurred by the contractor by reason of their compliance with this provision shall be considered as completely covered by the unit prices bid for inlets, pipes and flared end sections.

The Bidder shall include the following Job Special Provision.

**LOW-TRACKING OR NON-TRACKING TACK COAT NJSP-15-15C**

**1.0 Description.** This work shall consist of preparing and treating an existing bituminous or concrete surface with a low-tracking or non-tracking tack coat material prior to an asphalt overlay in accordance with Section 407, except as revised by this specification.

**2.0 Material Requirements.** All material shall be in accordance with Section 1015 of the Standard Specifications and specifically as follows:

<b>Emulsion Properties for Low-Tracking or Non-Tracking Tack Coat</b>			
Test on Emulsion	Method	Min	Max
Viscosity, Saybolt Furol @ 25°C (77°F), s	AASHTO T 59	20	100
Particle Charge Test		--	
Storage Stability Test <sup>a</sup> , 24 hr, percent	AASHTO T 59	--	1.0
Sieve Test, percent	AASHTO T 59	--	0.30
Residue by Distillation, percent	AASHTO T 59	50	
Oil Distillate by Distillation, percent	AASHTO T 59	--	1
<b>Test on Residue from Distillation</b>			
Softening Point, °F	AASHTO T 53	149	200
Penetration 25°C, 100 g, 5 s	AASHTO T 49	--	40
G* / sin delta @ 76° C – 10 rad/sec, kPa	AASHTO T 315	1.0	--
Solubility in Trichloroethylene <sup>b</sup> , %	AASHTO T 44	97.5	--

<sup>a</sup> In addition to AASHTO T 59, upon examination of the test cylinder, and after standing undisturbed for 24 hours, the surface shall show no appreciable white, milky colored substance and shall be homogeneous brown color throughout. The storage stability test may be waived provided the asphalt emulsion storage tank at the project site has adequate provisions for circulating the entire contents of the tank, provided satisfactory field results are obtained.

<sup>b</sup> In lieu of performing AASHTO T 44, AASHTO T 111, Ash in Bituminous Material, may be performed with a maximum allowable percent ash of 1.0 percent.

**2.1 Low-Tracking or Non-Tracking Requirements.** In addition to the above Material Requirements, low-tracking or non-tracking tack shall not stick to the tires, tracks or other parts of paving equipment or vehicles such that the surface to be overlaid becomes visible or void of tack prior to the placement of the asphaltic concrete pavement mixture. The tack material shall exhibit a low-tracking or non-tracking characteristic within 20 minutes of being applied to the roadway. If a tack coat material is unable to satisfy these conditions, then a polymer modified emulsion membrane as described under the Optional Polymer Modified Emulsion Membrane section of this JSP shall be used.

**3.0 Equipment and Construction Requirements.** All equipment and construction requirements shall be in accordance with Section 407; except as revised as follows:

**3.1 Weather Limitations.** The low-tracking or non-tracking tack coat shall not be placed on any wet surface or when the ambient temperature or the temperature of the pavement on which it is to be placed is below 50° F. Temperatures shall be obtained in accordance with MoDOT Test Method TM 20.

**3.2 Spraying Temperature.** The low-tracking or non-tracking tack coat emulsion shall be applied at temperatures between 160° F and 180° F. Temperatures of the tack shall not exceed 180° F and any overheated material shall be rejected.

**3.3 Storage and Handling.** All guidelines and instructions about storage and handling of the non-tracking tack product shall be followed in accordance with the product manufacturer.

**3.4 Distributor.** The distributor shall have the full circulating and heating capabilities in the tank. If the particle charge of the low-tracking or non-tracking tack is different from the particle charge of the emulsion that was previously used then the tank shall be thoroughly cleaned prior to use, since some products are not compatible. The following heating and circulating process shall be used:

- 1) The emulsion shall be slowly heated to 140° F.
- 2) Begin circulating the emulsion in the distributor tank only (100 to 150 gallons per minute) and continue slowly increasing heat to 160° F to 180° F.
- 3) Once the desired temperature is reached, begin circulation in the distributor bar.
- 4) Maintain circulation in the distributor's spray bar for a minimum of 30 minutes prior to tack application.

**3.5 Curing.** The low-tracking or non-tracking tack shall be allowed to cure prior to any construction traffic driving on the surface. A minimum of 15 minutes of cure time shall be allowed prior to driving on the tacked surface, unless less cure time is successfully demonstrated and approved by the engineer.

**3.6 Supplier Information.** The low-tracking or non-tracking tack materials are a different type of product compared to the conventional tacked used in Missouri. The following manufacturers are known producers/suppliers of low-tracking or non-tracking tack products:

Blacklidge Emulsions, Inc.  
Calumet Specialty Product Partners, L.P.  
Heartland Asphalt Materials  
Vance Brothers

There may be other manufacturer's that can produce an equivalent product. All products that are in compliance with this specification will be allowed.

#### **4.0 Optional Polymer Modified Emulsion Membrane.**

**4.1 Description.** In lieu of using a low-tracking or non-tracking tack coat material, a Polymer Modified Emulsion Membrane may be placed prior to a bituminous overlay of hot asphaltic concrete pavement. The Polymer Modified Emulsion Membrane shall be spray applied immediately prior to the application of the hot asphaltic concrete pavement so as to produce a homogeneous surface in accordance with Secs 401, 402, or 403.

**4.2 Materials.** The Polymer Modified Emulsion Membrane shall be in accordance with Sec 1015.20.5.1.1 or Sec 1015.20.6.2.

**4.3 Construction Requirements.** The asphaltic concrete pavement shall be placed in accordance with Secs 401, 402, or 403, except as modified herein.

**4.4 Equipment.** No wheel, track or other part of the paving machine or any hauling equipment shall come in contact with the Polymer Modified Emulsion Membrane before the asphaltic concrete pavement mixture is applied.

**4.5 Application of Polymer Modified Emulsion Membrane.**

**4.5.1** The Polymer Modified Emulsion Membrane shall be sprayed at a temperature of 120 - 180° F. The sprayer shall accurately and continuously monitor the application rate and provide a uniform coverage across the entire width to be overlaid. The target application rate of the asphalt emulsion membrane shall be within  $\pm 0.02$  gallon per square yard of the target application rate indicated on the project plans. The Engineer may make adjustments to the application rate based upon the existing pavement surface conditions and the recommendations of the Polymer Modified Emulsion Membrane supplier.

**4.5.2** Water may be added to SS-1hp and CSS-1hp by the emulsion manufacturer and shipped to the jobsite. No dilution shall be allowed in the field. When water is added to SS-1HP or CSS-1HP, the resulting mixture shall contain no more than 20 percent of added water. The contractor shall notify the engineer of the use of a diluted emulsion. The exact quantity of added water shall be indicated on the manufacturer's bill of lading, manifest or truck ticket. The application rate of the resulting mixture shall be adjusted such that the original emulsion will be spread at the specified rate. No water shall be added to the CPEM-1 or PEM-1.

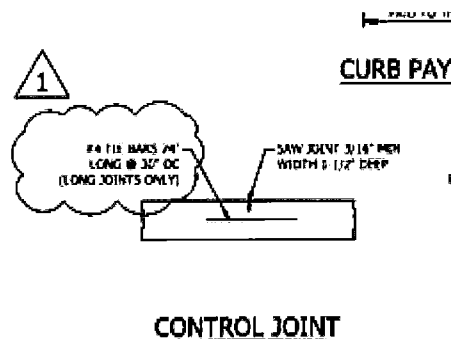
**5.0 Basis of Payment.** This work shall be considered incidental to the placement of the asphalt. All costs associated with this work shall be considered completely covered by the bituminous pavement pay items in the contract.

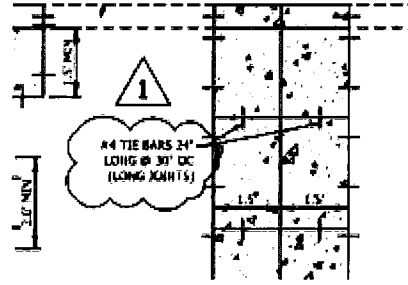
**PLAN SHEET CHANGES:**

The Bidder shall acknowledge the following plan sheet revisions:

**Sheet 3:**

Revised notes on joint details to show tie bars at all control and longitudinal joints.





Pavement repair notes were adjusted.

NOTES:

1. JOINTS SHALL BE LOCATED PER CITY OF JEFFERSON STANDARD DRAWING 10-11
2. TRANSVERSE JOINTS SHALL BE SAMPED AND GRABED PER 10-11
3. LONGITUDINAL JOINTS SHALL BE SAMPED AND GRABED PER 10-11
4. ALL JOINTS SHALL BE FINISHED WITH A 24" LONG TIE BAR
5. ALL JOINTS SHALL BE FINISHED WITH A 24" LONG TIE BAR
6. ALL JOINTS SHALL BE FINISHED WITH A 24" LONG TIE BAR
7. ALL JOINTS SHALL BE FINISHED WITH A 24" LONG TIE BAR



Quantity table was updated to reflect the changes noted in the bid proposal.

Item No.	Description	Total Quantity	Unit
<b>Base Bid</b>			
201 00 01	Laboring and Loading	1	LS
202 20 02	Removal of Improvements	1	LS
203 00 01	Formwork	1	LS
204 21 00	Class II Sawdust in Mass	517	CY
204 50 03	Weather Encasement	1,290	LF
207 00 00	Travel Loading for Material	1.0	STA
210 00 00	Available for Standard Budgets	3,466	SY
210 00 01	Standard Budgets	1,779	CY
304 00 00	Aggregate Base Mass	10,767	SY
305 00 04	Variable Depth Asphalt Mill	499	SY
305 00 05	1 1/2" Asphalt Mill	909	SY
305 00 06	Bituminous Pavement - 1 1/2" Base	14,438	SY
305 00 07	Bituminous Pavement - 1 1/2" Surface	10,512	SY
305 00 08	Concrete Pavement (12')	2,800	SY
305 00 09	Bituminous Pavement - Variable Depth Surface	431	SY
413 00 00	PCC Joints/Block Seal	2,202	LF
413 00 01	1/2" of Block	1	EA
403 00 23	8" SDR 30 PVC Pipe	184	LF
403 00 24	8" 1/2" SDR 35 PVC Pipe	192	LF
403 00 28	1" Dia. Manhole	3	EA
405 00 01	Underdrain	1,229	LF
405 00 02	Yard Drain	25	LF
405 00 03	Open-end Fence (20' in.)	75	LF
405 00 04	Concrete Kickback - 6 in.	23	SY
405 00 05	8" PCC Residential Curbside Drive	1,830	SY
405 00 06	8" PCC Residential Concrete Lane Approach	500	SY
405 00 07	Type A Curb and Gutter	8,480	LF
405 00 08	Integral Curb	1,270	LF
411 30 00	Type 2 Non-slip Marked (24' Block)	184	CY
414 00 01	Construction Signage/Traffic Control	1	LS
414 00 02	Mobilization	1	LN
414 20 00	Class B Concrete (Retaining Walls)	7	CY
414 40 01	Class B 1" Concrete (1-Joiner)	248	CY
704 00 00	Reinforcing Steel (1,000's)	10,180	LN
704 00 01	Reinforcing Steel (Retaining Wall)	1,400	LN
704 00 02	12" Class III Reinforced Concrete Pipe (Under)	1,640	LF
704 00 03	18" Class III Reinforced Concrete Pipe (Under)	24	LF
704 00 04	30" Class III Reinforced Concrete Pipe (Under)	184	LF
710 00 00	Precast Concrete Manhole - 48 in.	1	EA
710 00 01	Precast Concrete Manhole - 72 in.	1	EA
710 00 02	Type A Concrete Curb Inlet 4 x 4	1	EA
710 00 03	Type A Concrete Curb Inlet 4 x 6	1	EA
710 00 04	Type C Concrete Curb Inlet 4 x 4	12	EA
710 00 05	Type C Concrete Curb Inlet 4 x 6	2	EA
710 00 06	Type A Concrete Curb Inlet 4 x 8	1	EA
710 00 07	Area Inlet 4 x 4	1	EA
710 00 08	15' R.P. End Section	1	EA
805 00 01	Seedling, Fertilizing and Mutch	1.1	AC
805 00 02	Temporary Seeding and Mulching	1.1	AC
805 00 03	Soil Tests	791	LF
805 00 04	Plant Mark	28	EA
805 00 05	Topsoil/Planting Mix	14	SY
<b>Add Alternate A</b>			
422 00 00	Formwork (Including Formwork Removal)	2,040	SY
<b>Add Alternate B</b>			
413 00 00	PCC Joints/Block Seal	2,990	LF
<b>Total Add Alternate B</b>			

**QUANTITY NOTES:**

- 1 QUANTITIES FOR DRIVEWAY APPROACH AND DRIVEWAYS INCLUDE ALL ITEMS IN THE SUBDIVISION ADJACENT TO STREET PAVEMENT REPLACEMENT WHETHER SHOWN FOR REPLACEMENT OR NOT.



**DOCUMENTS ATTACHED FOR REFERENCE**

Updated Bid Proposal Form  
Minutes from Pre-Bid Meeting held February 23, 2018.  
Updated Plan Sheet 3  
Updated Plan Sheet 38

**BARTLETT & WEST, INC.**



Austin Johnson, P.E.  
Project Engineer

Enclosures



**MEADOWBROOK ESTATES SUBDIVISION  
STORMWATER IMPROVEMENTS  
ITEMIZED PROPOSAL**

Item No.	Item Description	Unit	Quantity	\$ Unit Price	\$ Amount
201-99.01	Clearing and Grubbing	LS	1		
202-20.10	Removal of Improvements	LS	1		
203-99.01	Earthwork	LS	1		
206-31.00	Class 3 Excavation in Rock	CY	107		
206-99.03	Waterline Excavation	LF	1,290		
207-99.09	Linear Grading for Stream	STA	1.8		
210-99.05	Geotextile Fabric for Stabilized Subgrade	SY	3,558		
210-99.07	Stabilized Subgrade	CY	1,779		
304-99.05	3" Aggregate Stone Base	SY	16,765		
401-99.04	Variable Depth Asphalt Mill	SY	466		
401-99.05	1.5" Asphalt Mill	SY	906		
401-99.06	Bituminous Pavement - 3.5" Base	SY	14,406		
401-99.07	Bituminous Pavement - 1.5" Surface	SY	15,537		
401-99.08	Concrete Pavement (6")	SY	2,830		
401-99.09	Bituminous Pavement - 1.5" Surface	TON	431		
413-60.00	PCCP Joint/Crack Seal	LF	7,202		
603-99.22	Thrust Block	EA	3		
603-99.23	8" SDR 35 PVC Pipe	LF	184		
603-99.24	8" Ductile Iron Force Main	LF	195		
603-99.28	4' Dia. Manhole	EA	3		
605-99.03	Underdrain	LF	1,229		
605-99.23	Yard Drain	LF	35		
607-10.11A	Chain-Link Fence (48 in.)	LF	78		
608-60.04	Concrete Sidewalk, 4 in.	SY	23		
608-99.05	6" PCC Residential Concrete Drive	SY	1,830		
608-99.15	6" PCC Residential Concrete Drive Approach	SY	655		
609-99.03	Type A Curb and Gutter	LF	8,460		
609-99.23	Integral Curb	LF	1,273		
611-30.99	Type 2 Rock Blanket (24" thick)	CY	184		
616-99.01	Construction Signage/Traffic Control	LS	1		
618-10.00	Mobilization	LS	1		
703-20.09	Class B Concrete (Retaining Walls)	CY	7		
703-40.41	Class B-1 Concrete (Culverts)	CY	248.8		
706-10.30	Reinforcing Steel (Culverts)	LB	56,160		
706-10.40	Reinforcing Steel (Retaining Wall)	LB	1,400		
726-13.15	15" Class III Reinf. Concrete Pipe Culvert	LF	1,642		
726-13.18	18" Class III Reinf. Concrete Pipe Culvert	LF	21		
726-13.30	30" Class III Reinf. Concrete Pipe Culvert	LF	184		

Item No.	Item Description	Unit	Quantity	\$ Unit Price	\$ Amount
731-00.48	Precast Concrete Manhole - 48 in.	EA	1		
731-00.72	Precast Concrete Manhole - 72 in.	EA	1		
731-99.01	Type A Concrete Curb Inlet 4' x 3'	EA	4		
731-99.02	Type A Concrete Curb Inlet 4' x 4'	EA	1		
731-99.06	Type C Concrete Curb Inlet 4' x 4'	EA	12		
731-99.07	Type C Concrete Curb Inlet 6' x 4'	EA	2		
731-99.08	Type C Concrete Curb Inlet 6' x 5'	EA	1		
731-99.09	Area Inlet 3' x 3'	EA	1		
732-99.01	15" RCP End Section	EA	1		
805-99.01	Seeding, Fertilizing and Mulch	AC	1.1		
806-10.17	Temporary Seeding and Mulching	AC	1.1		
806-10.19	Silt Fence	LF	791		
806-10.25	Inlet Check	EA	28		
806-99.05	Turf Reinforcement Mat	SY	14		
<b>TOTAL BASE BID:</b>					

**ADD ALTERNATE A**

Item No.	Item Description	Unit	Quantity	\$ Unit Price	\$ Amount
622-30.10	Diamond Grinding (Concrete Pavement)	SY	2,042		
<b>TOTAL ADD ALTERNATE A:</b>					

**ADD ALTERNATE B**

Item No.	Item Description	Unit	Quantity	\$ Unit Price	\$ Amount
413-60.00	PCC Joint/Crack Seal	LF	2,950		
<b>TOTAL ADD ALTERNATE B:</b>					

<b>TOTAL BASE BID PLUS ADD ALTERNATE A:</b>					
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BIDDER recognizes and acknowledges the receipt of the following Addenda:

Date      Addendum Number                      Date   Addendum Number

\_\_\_\_\_

\_\_\_\_\_

If the Bidder intends to use any subcontractor in the course of the construction, he shall list them. (If necessary, attach additional pages to list all subcontractors.)

Company Name	Address	City, State, Zip	Work To Be Performed	% of Bid

**Meadowbrook Estates Subdivision  
Stormwater Improvements  
Project No. 2016-201-1**

**Pre-Bid Meeting Notes  
February 22, 2018**

The meeting was held at the Cole County Fire Protection District "Training and Administration Building" beginning at 9:00am. Those attending the meeting are listed below:

<u>Name</u>	<u>Representing</u>	<u>Phone</u>
Eric Landwehr	Cole County	573-636-3614
Spencer Coonce	Cole County	573-636-3614
Austin Johnson	Bartlett & West	573-634-3181
Jeff Wankum	City of Jefferson	573-291-8284
Pat Daugherty	Lamke Trenching Inc.	314-581-7251
Marty Masters	MMLC	573-680-6954
Luke Hake	Stockman Construction	573-635-1316
Quintin Matheis	Jefferson Asphalt	573-418-9997
Mike Huff	Capital Paving	573-636-7121
Ron Helmig	Aplex, Inc.	573-680-0589
Bryant Gaines	Sam Gaines Construction	573-257-6123
Mike Schrage	BRS Construction	660-341-4665
Josh Bishop	JD Bishop Construction	660-752-5012
Randy Kay	PWSD#2	573-690-8578
John Kirchhoff	Lehman Construction	573-796-8101

**GENERAL BIDDING INFORMATION**

1. Sealed proposals on the forms prepared by the Cole County Department of Public Works will be received by the County Commission, Cole County, Missouri, at the Courthouse Annex, Jefferson City, Missouri, until **9:00 a.m., on March 2, 2018**, for the **Meadowbrook Estates Subdivision Stormwater Improvements Project**. The proposals will be publicly opened and read aloud thereafter. Bids received after the specified time for opening will be returned unopened.
2. Plans and specifications may be viewed online in the bids section at [www.colecounty.org](http://www.colecounty.org). All contractors wishing to bid on this project shall obtain an official copy of the plans and specifications at the office of the Cole County Department of Public Works, 5055 Monticello Road, Jefferson City, Missouri 65109-9182, (573) 636-3614.
3. **All prospective bidders will be required to purchase a set of plans and specifications in order to be able to bid this project. If a bid is submitted without purchasing a set of plans and specifications, the bid will be rejected.** This requirement is necessary to ensure that all of the bidders have received the applicable addenda before submitting a bid for this project.
4. A bid guaranty will be required in the amount of 5% of the bid. Can be paper bid bond or cashier's check.
5. Questions regarding the plans and specifications will be taken up to **24 hours prior to the date and time of the bid opening**. This will allow the County to get the information out to all of the bidders. General questions regarding the bidding process will be answered up to

## Meadowbrook Estates Subdivision Stormwater Improvements

### Pre-Bid Minutes

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the time of the opening.

6. Prevailing wage rates are required on this project. State rates apply.
7. Bids shall be submitted on the proposal forms in the bidding documents. All blank spaces shall be filled in on the forms by typewriter or by handwriting in ink. The bidder shall make no other stipulations in the bid form or qualify his bid in any manner. The bids shall be submitted in an opaque sealed envelope marked "**Meadowbrook Estates Subdivision Stormwater Improvements Project**" and shall be clearly marked with project title, bid date, bid time and bidder's name and address.
8. If awarded the contract, the Contractor agrees to complete the work by no later than **December 14, 2018** as defined in the specifications.
9. **Liquidated damages** shall be **\$1,000.00** per calendar day for every day that the Contractor fails to complete the work as provided in the contract documents.
10. Simultaneously with the delivery of the executed contract, the Contractor shall furnish a surety bond in the amount of 100% of the total contract sum, as security for both faithful performance of the contract and for the payment of all persons performing labor on the project under this contract as specified in the contract documents.
11. Do NOT include federal excise tax or sales and use taxes in the bid prices. This project will be a SALES TAX EXEMPT project. A copy of the federal tax exemption certificate will be furnished if required.

#### **GENERAL PROJECT INFORMATION**

1. This is a large project with several inlets, junction boxes, pipes, two box culverts, sewer and water line work and a lot of curb and gutter and pavement work.
2. The contractor is highly advised to visit the project location to observe existing site conditions.
3. The County will be providing construction observation through county inspectors.
4. The County will provide construction staking.
5. Utilities – There are several utilities on this project. Some adjustments have already been made and some will be made during construction.
6. ALL CHANGE ORDERS – If there is any item that the contractor thinks additional pay is warranted, a change order request will have to be submitted and approved prior to commencing any of the additional work. Any extra work done without approval is not guaranteed to be paid.
7. Several properties have special notes and miscellaneous items that need to be addressed as part of the project.
8. Staging and sequencing will be important as the project progresses. The contractor will need to consider access to properties as they plan out how to build the project.
9. Trackless Tack will be required and a new JSP will be added in the addendum.
10. If we find a lot of base rock underneath the existing pavement we may do away with the new 3" base and use the existing. If we do this we will work out a new price to account for machine time and labor to grade the existing base. This will be a variable item as the project progresses.
11. There is a USACE 404 permit for this project. The contractor shall follow all provisions.
12. The county inspector will assist with the Land Disturbance Permit BMP inspections after rainfalls. This is not specifically called out in the JSP.
13. There is some sewer line replacement work near the box culvert on Lake Valley Dr. A gravity main and force main will be relocated due to conflict with the new box culvert. The

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- new force main needs to be constructed and wait until a set time to do the tie-ins. The contractor will have about 4 hours to do the tie-ins. That is the approximate amount of time the pump station below would be able to hold the sewage.
14. There is water line excavation work on Cheryl Drive and a little on Cheryl Dr. PWSD2 will place the line, but the contractor will do the excavation and backfilling work and place granular bedding and backfill.
  15. Diamond Grinding on Lake Valley Drive and Ridgefield Drive is an add alternate. We'll see what kind of price we get and make a decision on whether to include this.
  16. Meadowview Drive is being narrowed by 2' to avoid waterline conflicts. This may not be obvious when looking at the plans.
  17. There has been no one contact us about wanting the excess haul off material but we believe there are a few places very close by that would be good locations.
  18. A contact for a diamond grinding subcontractor is:  
Scott Faltemier  
314-581-3539 Mobile  
636-432-1144 Office  
grinderworksinc@gmail.com

### **STANDARD PLANS AND SPECIFICATIONS**

Technical specifications will be the latest effective **2016** version of the Missouri Standard Specifications for Highway Construction. City of Jefferson City standard drawings shall apply to most items and MoDOT specifications shall apply to the rest. These are listed in the JSP's.

### **JOB SPECIAL PROVISIONS**

The job special provisions were reviewed. Special attention was given to the following:

1. **B. Work Zone Traffic Management.** A three week notice shall be given for all road closures. Contractor must coordinate with property owners to schedule driveway construction and provide a reasonable access to their property. Idlewood Road shall be open at all times but can be one lane if a flagger is used.
2. **D. Utilities.** Contractor shall coordinate with all utility companies regarding any relocation or special caution when working around the utilities. Sewer and water work will be done with this contract as shown in the plans and specified in this jsp.
3. **H. Excavation for Waterline Replacement.** The contractor shall provide the excavation and backfill for PWSD2 waterline replacement on Cheryl Drive and Lake Valley Drive. The water district will provide the labor and materials for the new waterline. The location on Cheryl Drive will be parallel to the storm sewer west of Christine Drive and behind the curb east of Christine Dr. There is 40' on Cheryl that the water district will bore under a retaining wall and this length is excluded from the quantity in the plans.
4. **I. Temporary Seeding.** This shall be used if seeding is required outside the growing season or for erosion control on rough grading.
5. **J. Seeding, Fertilizing and Mulch.** This is our basic JSP but one thing to point out is that final grade shall be established and seeded within 10 working days of proposed improvements being completed on a property unless otherwise approved by the engineer. The idea behind this is that we don't want a property left torn up for an extended time period.
6. **K. Clearing and Grubbing.** This work consists of all clearing, grubbing, removal of trees,

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- brush or objectionable vegetation within the ROW or easements necessary for the completion of the project. Some tree removals are shown on the plans and some are not. The engineer will mark the ones to be removed.
7. L. Curb Profile Adjustments During Construction. During this type of work the curb profiles are usually adjusted a little up or down from existing. In some locations it is shown on the plans where we are making significant changes. Other locations not specifically called out will be adjusted as needed by the engineer. Any changes in excavation quantities of the roadway pavement and subgrade are incidental to the project and no pay adjustments are warranted.
  8. N. Removal and Replacement of Landscaping Features. The contractor shall coordinate with property owners regarding the landscaping that might be impacted by construction. If necessary the contractor may need to help with some basic removal of some items.
  9. P. Permits. The county has a land disturbance permit through DNR. A county Storm Water Pollution Prevention Plan will be used and reports shall be done by the county inspector and contractor foreman. There is also a USACE 404 Permit acquired the project.
  10. T. Roof Drains and Foundation Drains. All drains shall be connected regardless of whether they show on the plans or not. Preference is to tie into an inlet.
  11. X. Sanitary Sewer Construction. All work shall follow the City of Jefferson standard details. An inspector for the city shall be present during sanitary sewer work.
  12. Y. Removal of Improvements. All removals necessary for the completion of the project are included in this bid item. Typical removals are noted but are not limited to this list.
  13. Z. Pavement Removal. Specifics are listed here for the types of pavement to be removed and the methods. Costs shall be included in the Lump Sum Removal of Improvements bid item.
  14. AA thru HH. These are the asphalt job special provisions.
  15. II. Paving Requirement Around Manhole Lids. This addresses the matching of the pavement to the top of the lids.
  16. JJ. Centerline Joint. This provision addresses the asphalt centerline joint between lanes to ensure that it is reasonably straight.
  17. LL. Removal and Replacement of Poor Subgrade Material. There is a potential to encounter some bad subgrade on some of these roads. This provision specifies the replacement requirements.
  18. NN. Extra Quantity for Specific Bid Items. Extra quantities for concrete pavement and concrete driveway has been added to the bid quantity. These items typically overrun.
  19. OO. Roadway Excavation. Earthwork will be paid per lump sum. The typical earthwork items are listed but not limited to what is shown in this JSP. Depth of rock was investigated at the inlets for class 3 excavation in rock and a quantity is included in the plans to be paid for per cubic yard.
  20. PP. Linear Grading for Stream. The stream between Hwy 179 and Cheryl Drive shall be graded for proper drainage and a scour hole at the outlet of the Hwy 179 box culvert shall be filled in.
  21. RR. Underdrain. There are several locations on the plans where an underdrain or edge drain shall be installed behind the back of curb. Details for construction are shown in the plans.
  22. UU. Joint Seal. The new and existing joints in the concrete pavement on Courtyard Drive shall be sealed. Existing joints are to be cleaned out before sealing.

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### PLAN REVIEW

Eric Landwehr and Austin Johnson discussed the following items on the plan sheets:

1. There is a detail for the underdrain.
2. The overview sheets give a pretty good idea of the work involved.
3. All the asphalt streets will have the curb and gutter replacement and full depth pavement replacement EXCEPT Christine Dr. That will be left in place and a 1.5" mill and fill will be done.
4. Idlewood Road will have some variable depth milling to tie in on both ends and a 2" average thickness overlay. This is being done to get the road closer to a consistent cross slope.
5. There is a chance the county will overlay the rest of Idlewood Road to the bridge over Hwy 54. If so, the bid item will overrun accordingly.
6. There is a section of fence replacement on the Mans property along Idlewood.
7. Meadowview Drive will be narrowed by 2' to avoid water line conflicts. The side being moved in is the west side.
8. The inlets on Courtyard where the pavement replacement is being done are to be left in place. The inlet throats will be replaced for better drainage to the inlets.
9. There are a few easements we still need to get. One might be an issue. If we can't get an easement we'll just cut back our work to stay on ROW.
10. There is a manhole that will have the lid cast in the gutter pan. This was done to avoid having the lid in the street. The gutter pan will widen around the radius to make it work as shown on the plans.
11. Cheryl Drive will have some significant curb profile changes to make it more consistent.
12. Meadowview will have a section of the road all slope towards the west. This is primarily between Allison and Cheryl.
13. The plans show some curb and gutter as all darkened in. This is gutter that will slope towards the road and not the curb. The locations of this are part of Meadowview and at some radii of the intersections.
14. Pavement replacement on Courtyard Drive is shown on the plans. The areas shown for replacement are based on visual inspection. A decision on what gets replaced will be decided in the field and the plan locations will be the starting point.
15. There are detail sheets for the intersections with elevations.
16. The box culverts are MoDOT standard. The one on Lake Valley Drive has a section of retaining wall that will tie into the north side of the downstream wing wall.
17. The box culvert barrels can be precast. The ends (headwalls, wing walls, etc.) have to be cast in place.
18. Some driveways show a certain length back to be replaced and it will match into an existing joint. Some do not show anything and quantities in the plans assume 5'.

### QUESTIONS AND ANSWERS

Received at pre-bid meeting, by telephone, e-mail etc. prior to issuing Addendum No. 1

Q1. If the old asphalt is milled off, would you allow the millings to be used as base rock?

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- A1. No. They could be used under driveways since base rock is not specified for that. They could also be used for temporary driveway access. There may not be a whole lot of asphalt because the pavement underneath is concrete.
- Q2. For the waterline work, will the contractor have to expose all the utilities as they are going through and digging thus requiring a laborer with the operator?
- A2. It would be safe to assume you'll want to have a laborer with the operator as they are digging but it's not a requirement and the contractor can conduct the work as they see fit.
- Q3. Will the water district provide the granular backfill?
- A3. YES.
- Q4. Will the backfill need to be compacted?
- A4. Not like you would under the roadway but assume you'll need to do some compaction to minimize settling depending on the condition of the backfill material. Any significant settling will need to be fixed before final acceptance or during the warranty period.
- Q5. Will the contractor do the haul off of excess material from the waterline work?
- A5. YES
- Q6. Diamond grinding is expensive. Would you consider a mill and overlay of the pavement that has the diamond grinding alternate?
- A6. NO. The pavement is in good shape and the purpose is to just smooth out some bumps to help it match with the rest of the improved streets. If the price comes in too high for diamond grinding or if we are over budget, we won't do it.
- Q7. What kind of pipe is the sewer line?
- A7. Ductile Iron for the force main and truss pipe for the gravity main.
- Q8. Will we have to have a licensed plumber do the tie-ins for the sewer work?
- A8. NO. That only applies to service lines.
- Q9. Where is the access to the creek for the linear grading work?
- A9. From the properties on Cheryl Dr.
- Q10. For dump sites, can we take anywhere or do they have to be pre-approved?
- A10. As long as you are in the county you can take anywhere but if you take into the city, you have to follow their regulations.
- Q11. Are the inlet deflectors cast into the concrete?
- A11. YES
- Q12. Idlewood Road is supposed to be a variable depth thickness but the bid item is in SY. That will be hard to bid so what should we do?
- A12. We will do a separate bid item for the Idlewood Road piece and change it to Tons.
- Q13. Are the drop inlets supposed to be cast in place? What about the circular junction boxes/manholes?
- A13. All square/rectangular drop inlets or junction boxes shall be cast in place. Circular manholes can be pre-cast but you'll have to plan on being able to adjust the lid to match the appropriate height.
- A14. For the waterline work, any damage to existing utilities would be the responsibility of the contractor correct?
- A14. YES. If you are digging, you are responsible.
- Q15. Do you want to put the surface mix of the asphalt on at one time?
- A15. YES. Our intention is to do all the BP-1 be done towards the end at one time. We understand there may be circumstances where we need to do something ahead of

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time but let's try wait and do it all at once. It won't be an issue having a 1.5" lip for a while.

Q16. Can we get in there early and mill the asphalt off so we know what the condition of the concrete is underneath?

A16. That will be up to the contractor. We'd recommend that you NOT do all of the streets at once just because you may have to deal with fixing some pavement in areas you haven't started working yet. The pavement underneath could be in pretty bad shape.

ESL:el